



Village Core Buildings
Building Type I
Intent: Village Core Buildings are intended to serve the neighborhood with retail, restaurant, office and residential uses. Ground floors should have storefronts along all street facades.
Minimum Lot Area: N.A.
Minimum Lot Width at Front Setback: N.A.
Minimum Lot Depth: N.A..
Front Build-to Line: Where buildings directly front the public right of way, 80% of the front facade must be built to the front property line; where buildings front common areas (e.g. sidewalks, parking, open space), 80% of the front facade must be built to the back of the proposed sidewalk. Projections and recesses up to 2 feet for architectural articulation are permitted within the 80%.
Note: 100% of lot frontage should meet the build-to line requirement, excluding access drives (max. 62 feet between buildings) and pedestrian passages (max. 20 feet between buildings).
Note: For buildings fronting Nolensville Pike, 80% of the front building facade shall be built between 5ft. and 20 ft. of the front property line.
Minimum Side Yard Setback: None
Minimum Rear Yard Setback: None
Maximum Height: 4 stories
Minimum Height: 20 ft.
Minimum Ground Floor Height: 14 ft. floor to floor (floor to roof for one-story building)
Off Street Parking Spaces: None Required
Off Street Parking Location: Where provided, off street parking should be located behind buildings with the exception of single loaded access drives connecting to "C" streets as shown on the Design Plan-Street Network on page #6. Access drives to interior parking areas are permitted to connect to street types "G" and "E" provided they contain a maximum of two 12' drive lanes only.
Passages: Landscaped passages are encouraged between buildings to provide access from rear parking areas to the building fronts.
Note: The developer shall form a Design Review Committee. This committee will be charged with design review of all building designs to ensure compatibility and conformance with architectural design guidelines and covenants that do not fall under the purview of Metropolitan Government.

See Appendix for additional regulations.
Note: Images on this page are for visual reference only. No architectural designs have been finalized.

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Live/Work Buildings

Building Type II

Intent: Live/Work Buildings are intended to provide the opportunity to live directly above a workplace. Ground floors may have retail, office or residential uses.

Minimum Lot Area: N.A.

Minimum Lot Width at Front Setback: N.A.

Minimum Lot Depth: N.A..

Front Build-to Line: Where buildings directly front the public right-of-way, 80% of the front facade must be built to within 0 ft. and 15 ft. of the front property line; where buildings front common areas (e.g. sidewalks, parking, open space), 80 % of the front facade shall be built to within 0 ft. and 15 ft. of the back of the proposed sidewalk.

Note: 100% of lot frontage should meet the build-to line requirement, excluding access drives (max. 62 feet between buildings) and pedestrian passages (max. 20 feet between buildings).

Note: For buildings fronting Nolensville Pike, 80% of the front facade shall be built between 5 ft. and 20 ft. of the front property line.

Minimum Side Yard Setback: None

Minimum Rear Yard Setback: None

Maximum Height: 3-1/2 stories (3 stories plus occupied attic)

Minimum Height: 2 stories

Off Street Parking Spaces: None Required

Off Street Parking Location:

Where provided, off street parking should be located behind buildings with the exception of single loaded access drives connecting to "C" streets as shown on the Design Plan- Street Network on page #6. Access drives to interior parking areas are permitted to connect to street types "G" and "E" provided they contain a maximum of two 12' drive lanes only.

Passages: Landscaped passages are encouraged between buildings to provide access from rear parking areas to the building fronts.

Note: The developer shall form a Design Review Committee. This committee will be charged with design review of all building designs to ensure compatibility and conformance with architectural design guidelines and covenants that do not fall under the purview of Metropolitan Government.

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Multi-Family Buildings

Building Type III

Intent: Multi-Family buildings are intended to provide an opportunity for a more inclusive community. The desired goal is for renters to purchase property in the UDO over a period of time. Regulations and guidelines for Multi-Family buildings include clubhouses and accessory structures.

Minimum Lot Area: None
Minimum Lot Width at Front Setback: None
Minimum Lot Depth: None
Front Build-to Line: Where buildings directly front the public right of way, 80% of the front facade must be built to between 5 and 10 ft. from the front property line; where buildings front common areas (e.g. sidewalks, parking, open space), 80% of the front facade must be built to between 5 and 10 ft. from the back of the proposed sidewalk. Projections and recesses up to 2 feet for architectural articulation are permitted within the 80%.
Note: 100% of lot frontage should meet the build-to line requirement, excluding access drives (max. 62 feet between buildings) and pedestrian passages (max. 20 feet between buildings).

Note: Buildings fronting Nolensville Pike, 80% of the front facade shall be built between 5 ft. and 20 ft. of the front property line.

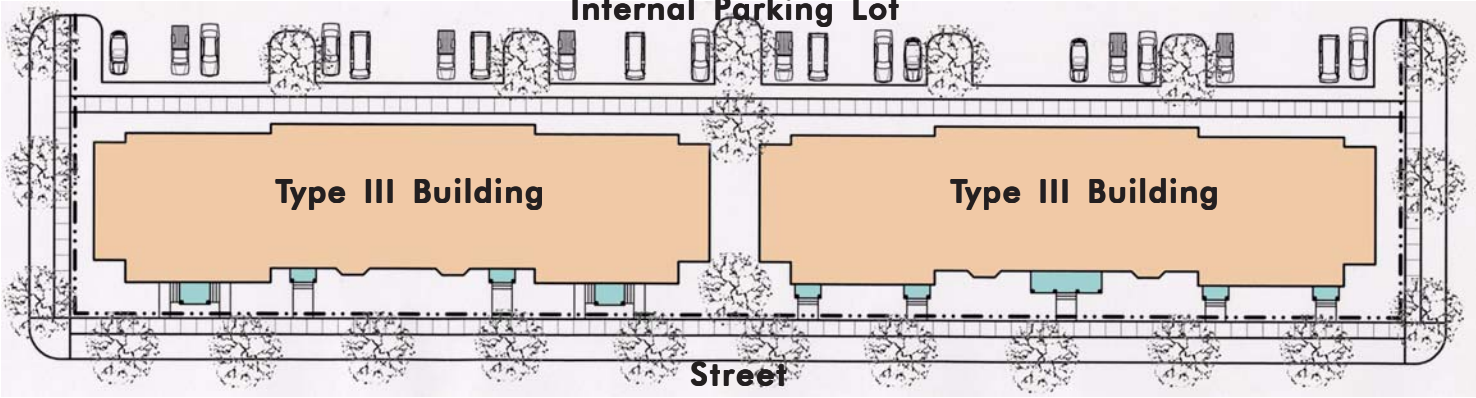
Minimum Side Yard Setback: 10 ft.
Minimum Rear Yard Setback: None
Maximum Height: 4 stories
Minimum Height: 2 stories
Minimum Raised Foundation: 12 inches
Off Street Parking Spaces: None Required
Off Street Parking Location:

Where provided, off street parking should be located behind buildings with the exception of single loaded access drives connecting to “B” and “C” streets as shown on the Design Plan- Street Network on page #6. Access drives to interior parking areas are permitted to connect to street types “G” and “E” provided they contain a maximum of two 12’ drive lanes only.

Passages: Landscaped passages are encouraged between buildings to provide access from rear parking areas to the building fronts.

Note: The developer shall form a Design Review Committee. This committee will be charged with design review of all building designs to ensure compatibility and conformance with architectural design guidelines and covenants that do not fall under the purview of Metropolitan Government.
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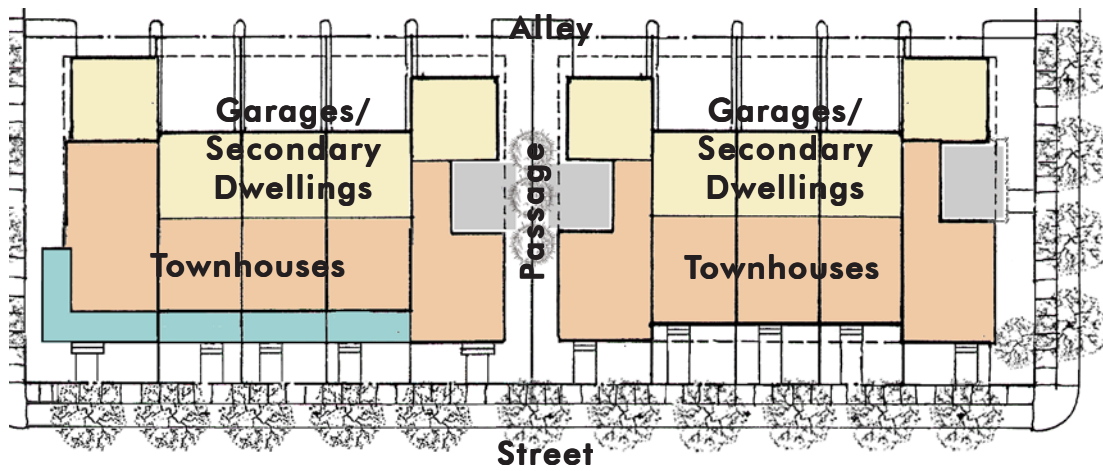


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Townhouses - Single Family Attached Building Type IV

Maximum Units Per Building: 10

Minimum Lot Area: 1,080 sq. ft.

Minimum Lot Width at Front Setback: 18 ft.

Minimum Lot Depth: 60 ft.

Front Yard Build-to Line: 5-15 ft.

Minimum Side Yard Setback: 5 ft.

Minimum Rear Yard Setback: 5 ft.; garages must be setback from the rear property line either 5 ft. or greater than 15 ft.; garages on parcels at the end of a block must have a garage building which is built to 5 ft. from the rear property line.

Maximum Height: 35 ft. to mean height of roof.

Minimum Raised Foundation: 18 in. except where front building wall is 10 ft. or less from the property line it shall be 24 in.

Off Street Parking Spaces: Two parking spaces per unit plus one parking space per Secondary Dwelling shall be provided. Tandem parking behind a garage space shall be counted toward meeting these requirements.

Off Street Parking Location: Off street parking and garage access shall be from a rear alley or lane. Garage doors should not face any streets.

Secondary Dwellings: A maximum of 25% of all Single Family dwellings (including townhouses) may have a Secondary Dwelling on the same lot.

Maximum Height: The maximum height of a secondary dwelling shall be equal to the height of the primary dwelling.

Passages: Common landscaped passages are encouraged between buildings to provide outdoor front to back access.

Note: The developer shall form a Design Review Committee. This committee will be charged with design review of all building designs to ensure compatibility and conformance with architectural design guidelines and covenants that do not fall under the purview of Metropolitan Government.

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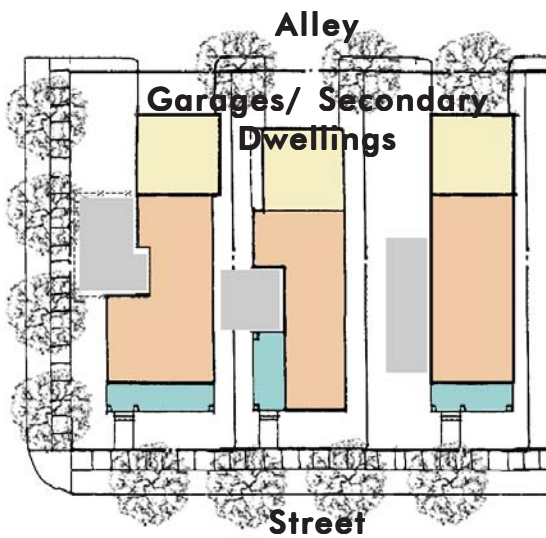
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Single Family Detached - Rear Access

Building Type V

Minimum Lot Area: 3,600 sq. ft.
Minimum Lot Width at Front Setback: 36 ft.
Minimum Lot Depth: 100 ft.
Front Yard Build-to Line: 5-25 ft.
Minimum Side Yard Setback: 3 ft.
Minimum Rear Yard Setback: 5 ft.; garages must be setback from the rear property line 5 ft. or greater than 15 ft; garages on parcels at the end of a block must have a garage building which is built to 5 ft. from the rear property line.
Maximum Height: 35 ft. to mean height of roof
Minimum Raised Foundation: 18 in. except where front building wall is 10 ft. or less from the property line it shall be 24 in.
Porches: Where houses have a covered front porch, they shall be a minimum of 6 ft. in depth excluding porches where the width is approximately the same as the steps as illustrated in top center image.
Off Street Parking Spaces: Two parking spaces per unit plus one parking space per Secondary Dwelling shall be provided. Tandem parking behind a garage space shall be permitted toward meeting these requirements.
Off Street Parking Location: Off street parking and garage access shall be from a rear alley or lane. Garage doors should not face any streets.
Secondary Dwellings: A maximum of 25% of Single Family Dwellings may have one Secondary Dwelling on the same lot.
Maximum Height: The maximum height of a secondary dwelling shall be equal to the height of the primary dwelling.

Note: The developer shall form a Design Review Committee. This committee will be charged with design review of all building designs to ensure compatibility and conformance with architectural design guidelines and covenants that do not fall under the purview of Metropolitan Government.

See Appendix for additional regulations.



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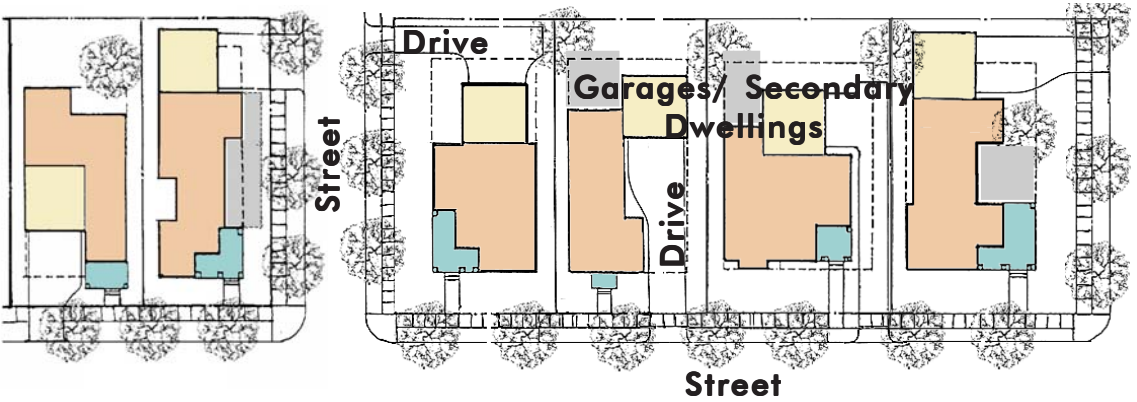
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Single Family Detached - Street Access

Building Type VI

Minimum Lot Area: 4,000 sq. ft. (Lot area is independent of min. lot dimensions due to irregular lot shapes.)
Minimum Lot Width at Front Setback: 50 ft.
Minimum Lot Depth: 100 ft.
Front Yard Build-to Line: 5-25 ft.
Minimum Side Yard Setback: 3 ft.
Minimum Rear Yard Setback: 10 ft.
Maximum Height: 35 ft. to mean height of roof
Minimum Raised Foundation: 18 in. except where front building wall is 10 ft. or less from the property line it shall be 24 in.
Porches: Where houses have a covered front porch, they shall be a minimum of 6 ft. in depth excluding porches where the width is approximately the same as the steps as illustrated in top left image.
Off Street Parking Spaces: Two parking spaces per unit plus one parking space per Secondary Dwelling shall be provided.
Off Street Parking Location: Garages must be recessed a minimum of 15 ft. from the front facade or porch. The back of parking pads must be recessed a minimum of 15 ft. from the front facade or porch. Driveways shall be limited to 12 ft. wide where they cross the front property line. Driveways may be from the side street.
Secondary Dwellings: A maximum of 25% of all Single Family Dwellings may have one Secondary Dwelling on the same lot.
Maximum Height: The maximum height of a secondary dwelling shall be equal to the height of the primary dwelling.



Note: The developer shall form a Design Review Committee. This committee will be charged with design review of all building designs to ensure compatibility and conformance with architectural design guidelines and covenants that do not fall under the purview of Metropolitan Government.
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Civic / Institutional Buildings

Building Type VII

Intent: Civic and Institutional buildings should be designed with prominence and monumentality. Key architectural features should act as community focal points. Where possible, street axes should be terminated by the primary building form or architectural feature. Towers, spires, porticos and other vertical forms are encouraged.

Front Yard Setback: Set by Design Review
Minimum Side Yard Setback: Set by Design Review
Minimum Rear Yard Setback: Set by Design Review
Maximum Height: Set by Design Review
Off Street Parking Spaces: None Required
Off Street Parking Location: The majority of parking should be located to the rear or sides of the buildings and should be screened with landscaping from streets, public open spaces and adjacent properties.

Note: The developer shall form a Design Review Committee. This committee will be charged with design review of all building designs to ensure compatibility and conformance with architectural design guidelines and covenants that do not fall under the purview of Metropolitan Government.



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Signage

Intent: Signage shall be of high quality design and craftsmanship and shall compliment the architecture. See the appendix for specific sign regulations.



Note: The developer shall form a Design Review Committee. This committee will be charged with design review of all signage designs to ensure compatibility and conformance with architectural design guidelines and covenants that do not fall under the purview of Metropolitan Government.

See Appendix for additional regulations.

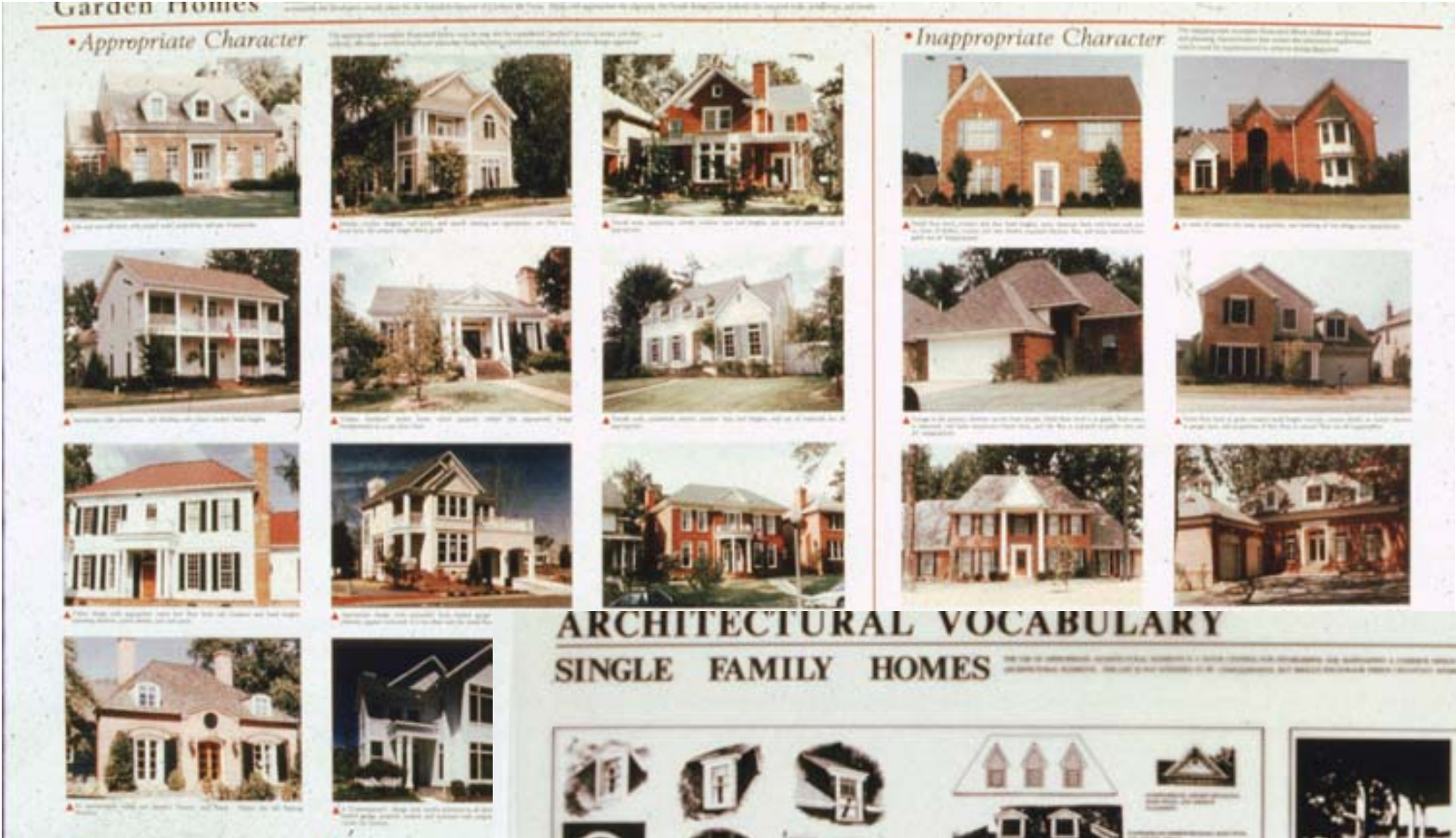
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Design Review

Final site plan review shall be in accordance with Article V of the zoning regulations.

In addition, the developer shall form a Design Review Committee. This committee will be charged with design review of all building designs to ensure compatibility and conformance with architectural design guidelines and covenants that do not fall under the purview of Metropolitan Government.

Design Guidelines will help insure successful integration of mixed-use buildings with ground level commercial and upper floor office and/or residential.

Attention will be paid to signage, awnings, seating and other details that are critical in making a place pedestrian friendly.

Decorative street lamps, shade trees and benches will help make streets and public open spaces feel enjoyable.

A design review process will ensure the use of proper proportions and details- key ingredients for timeless quality.

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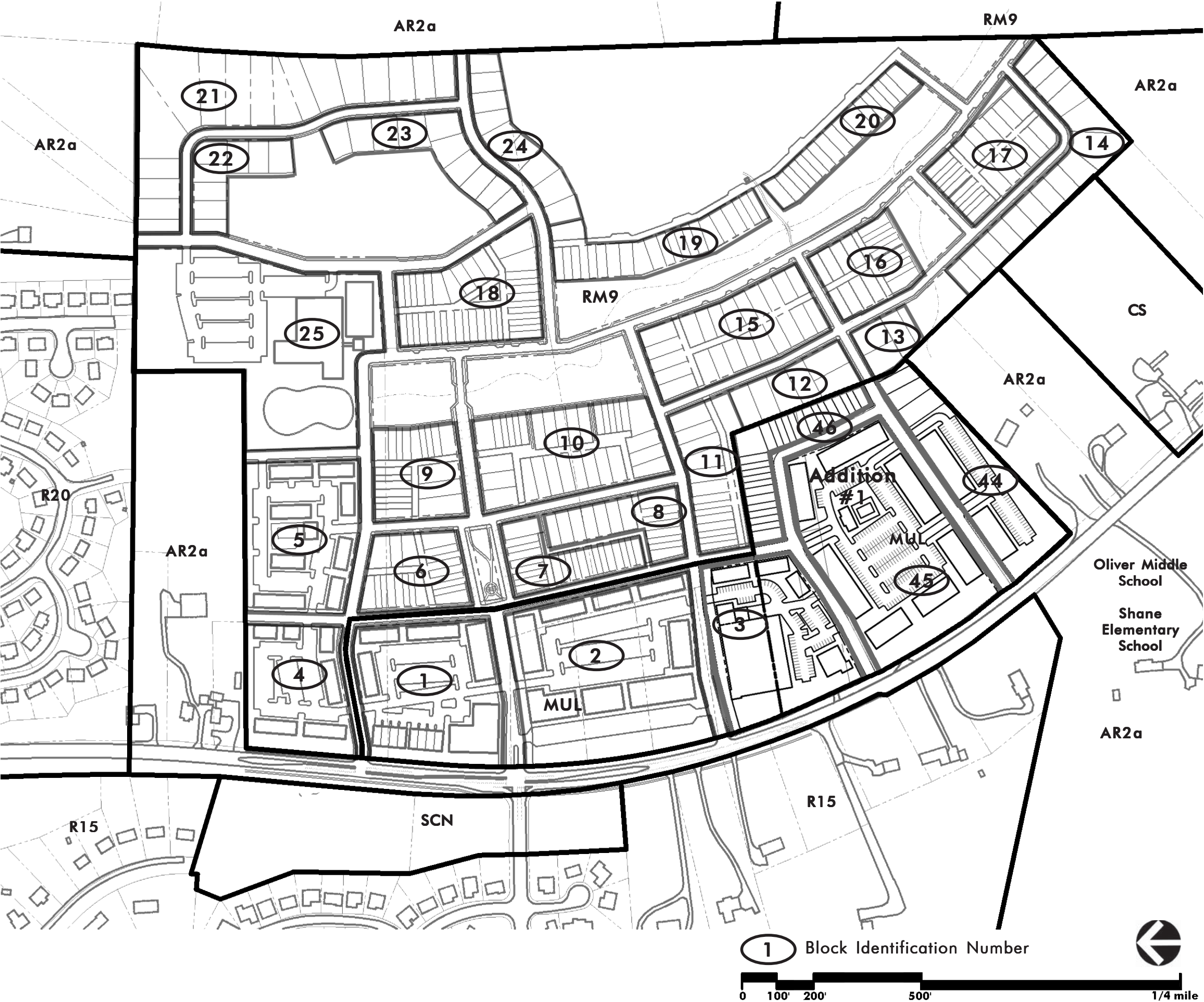
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Regulating Plan & Appendix





Regulating Plan

Total UDO Area: 208.04 acres
MUL Area: 35.43 acres
RM9 Area: 159.47 acres
RS10 Area: 13.14 acres
Total Maximum Residential Units (not including secondary dwellings): 1400 (includes multi-family and all single family attached and detached unit types; does not include residential units in live/work and village core buildings)
Maximum Multi-Family Units (not including Townhouses-Single Family Attached): 500
Open Space: Minimum 1 acre per 30 dwelling units.

- Notes:**
- The street and block layout illustrated on this plan are conceptual. Final construction plans may deviate from this layout as necessary to comply with required engineering standards and to make adjustments to market conditions.
 - Right-of-way dedication for future widening of Nolensville Pike shall be included on any final plat involving property that fronts Nolensville Pike in an amount sufficient to provide one half of the right-of-way required by the adopted major street plan in effect at the time the plat is filed for approval plus any additional right-of-way needed to accommodate left turn lanes at the three proposed intersections with Nolensville Pike.
 - Any final plat involving construction of segments of streets C or F that intersect Nolensville Pike shall provide for construction at developer's expense of the left turn lanes from Nolensville Pike and, in the case of street F, shall provide for the required pedestrian signal and crosswalk improvements.
 - If the Tennessee Department of Transportation approves the construction of a landscaped median in Nolensville Pike along the portion of the Pike that abuts Lenox Village, an agreement shall be negotiated between Metropolitan Government and the developers of properties along both sides of the affected section of Nolensville Pike to determine the proportionate sharing of design and construction cost among the developers along both sides. Final plats described above and for the frontage properties on the west side of Nolensville Pike shall provide for construction of the TDOT approved median at the negotiated proportionately shared expense of developers on both sides of the affected section of Nolensville Pike. The cost share for any one development for landscaping and irrigation shall not exceed \$40,000 (in year 2001 dollars).
 - The existing signal at Bradford Hills Drive shall be improved with pedestrian signals and the intersection shall be provided with ADA compliant cross walks across Nolensville Pike and the entrance road to Lenox Village.
 - Streets that are shown on the Street Network Design Plan for possible future extension to connect with streets on adjoining property shall, in the case of connection to existing streets, be connected at the time of construction only if Metropolitan Council has approved the connection.

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Regulating Plan
Addition #2

